

Market Square / Mixed-Use Office and Neighborhood Service District

The "Market Square" or "Mixed Use Office and Neighborhood Service District" is located in the center of the Leawood portion of the corridor and is bisected by Mission Road. This central portion of the corridor needs to be given special consideration, due to the unique relationship of Mission Road to the regional transportation system. Large scale retail uses should not be allowed to develop in this area. Instead, large scale retail developments should be limited to the portions of the corridor which have direct access to major north-south arterials such as Nall Avenue, Roe Avenue and State Line Road.

The purpose of the Market Square area is to provide a location for a collection of neighborhood scaled commercial and professional uses. The district will form a significant activity and shopping center for the residents of surrounding neighborhoods. A well designed area will serve surrounding neighborhood residents, encourage private investment into the area, and project a positive image of the 135th Street area to outsiders. A good mix of products and services will make living in surrounding neighborhoods more convenient and comfortable and a strong, attractive business district will add to the economic vitality and visual quality of the corridor.

Also, Market Square offers an opportunity to create a unique "Leawood" statement. The 135th Street corridor passes through four communities. The City of Leawood has the opportunity within the Market Square to establish a "Sense of Place" unique to the community which can be defined by developing specific design strategies. The strategies will address the following issues:

Nature of Land Uses. A well balanced mix of "neighborhood friendly" land uses will be encouraged in Market Square. The type, scale and quality of development in this sub-district all are important considerations.

135th Street Streetscape. The streetscape along 135th Street within Market Square will be different from the streetscape along other sections of 135th Street in Leawood and other communities. A common landscaping and streetscaping theme will be established to help define the corridor as unique.

Access Road Streetscape. Landscaping and streetscape elements along access roads, including reverse frontage roads will be established to emphasize the neighborhood scaled characteristic desired in the sub-district.

Intersection Design. Each intersection, including sidewalks, streetscape elements and landscaping will be designed to emphasize the neighborhood scale and unique nature of the district.

Integration of Parks and Plazas. Development in Market Square will feature the creation of neighborhood scaled parks, plazas and other amenities. These spaces will be tied into the pedestrian network of the sub-district as well as community wide pedestrian and bicycle paths.

Pedestrian Amenities. Guidelines for sidewalks, small plazas, street furniture and landscaping will help create a pleasant pedestrian environment.

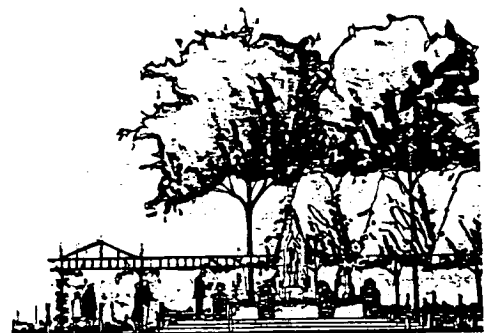
The following design standards establish standards for development in Market Square. The standards are divided into three general categories, streetscape, landscaping plans and property development. The "Streetscape" section specifies the nature of streetscape elements within Market Square. The "Landscaping Plan" section deals with the trees, shrubs and other plant materials required for individual property development. The standards outline how these landscaping elements are to be used for buffering, screening and interior landscaping. The "Property

Development" section outlines requirements for the *hardscape* elements, such as parking lots, sidewalks, lights and signs. The landscaping and other improvement requirements within the public right-of-way are addressed separately under the "streetscape" section of this document.

1. Streetscape Plans

A. 135th Street

Intent. The streets of the mixed-use office and neighborhood service district will be important public places which will serve the needs of vehicles and pedestrians. 135th Street will serve primarily as a major traffic arterial. The public right-of-way adjacent to the street is an important area for pedestrians and should be used to provide linkage between developments. Pedestrian needs should be carefully integrated into the streetscape system. Special attention should be paid to 135th Street. This street should receive concentrated design treatments, including street trees, signage elements, lighting and other amenities.



Corner of Mission/135th Street

Intersections

Gateway treatments are encouraged along arterials and neighborhood passageways where important focal points or centers of activity exist. These treatments may include signs, landscaping, monuments, lighting or other prominent design features.

The use of public art, fountains, and sculpture should be used at intersections and in public commons to help soften the auto-oriented nature of the area.

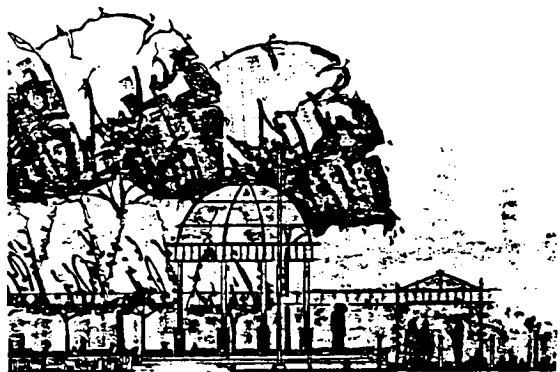
Street amenities should be clustered and well organized into "amenity zones" to provide comfortable and safe pedestrian spaces.

Street Median

The integrity of the median on 135th Street should be protected to help maintain the boulevard characteristics of the corridor. The street median should maintain its maximum designed width. Future street widening should not cut into the median.

Sidewalks

A continuous sidewalk should be provided along both



Corner of Mission/135th Street

sides of 135th Street. The sidewalk should be no less than 6 feet in width

The city should not widen 135th Street at the expense of the sidewalk and other pedestrian areas. Where street widening is necessary, trees, landscaping and sidewalks should be protected wherever possible and the negative impacts of widening should be mitigated. Pedestrian improvement along 135th Street should account for future widening of 135th Street.

The pedestrian walkway adjacent to 135th Street should be separated from the roadway by a tree lawn which is at least 4 feet in width. This separation should be maintained as the road is widened.

The sidewalk design should encourage the use of street furniture, tree planting, and landscaping in amenity zones, such as at street intersections where pedestrian crossing is to occur.

Mark crosswalks with a change of paving material, color, texture, or pattern which contrasts with the trafficway material to help define crosswalks as areas requiring special attention from both motorists and pedestrians.

Street Furniture

When possible elements of the streetscape such as lighting, traffic controls, signage, benches and other elements should have a consistent visual theme.

Signage

Design signage to be compatible with the surroundings and function.

Traffic control signs should be consolidated and located in an organized way and should be sized to be consistent with traffic speeds and other streetscape elements.

Utilities

When possible utilities should be kept underground. Above ground utility equipment, such as utility boxes within the street right-of-way shall be designed to blend into the street furniture design.

135th Street Landscaping

Tree planting and landscaping should be undertaken

along 135th Street as part of capital improvement projects.

Use street trees which create an image based on linear forms and become a design element of the streetscape.

Use street trees which mature to form shade canopies. Use trees of a similar species to provide consistency along a block face.

B. Reverse frontage and north-south roads

Intent. Reverse frontage roads and north-south access roads should be treated differently than 135th Street. These streets should be designed at a smaller, more neighborhood-friendly scale. Within the Market Square Sub-District vehicular traffic should be carefully controlled to provide a high quality pedestrian environment, and pedestrian needs should be carefully integrated into the streetscape system. These streets should receive design treatments, including street trees, signage elements, lighting and other amenities which emphasize the neighborhood character and scale desired in the area.

Intersections

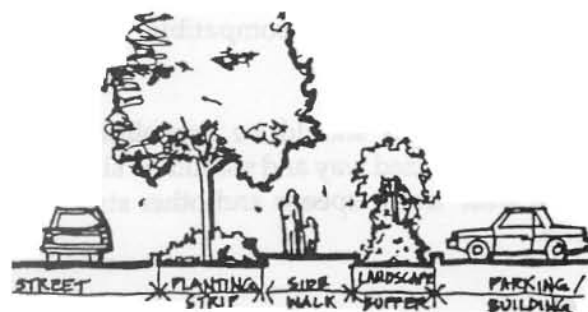
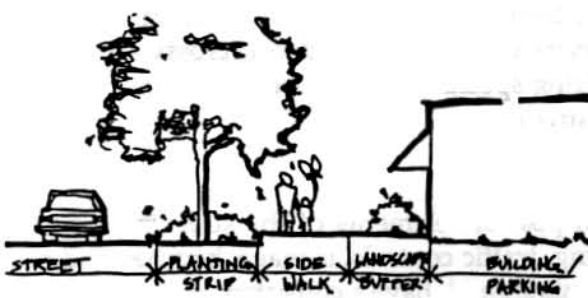
Gateway treatments should be constructed at street intersections which will emphasize the pedestrian scale of the district. The streetscape designs at these intersections should reflect the design of the 135th and Mission Intersection.

Street amenities should be clustered and well organized into "amenity zones" to provide comfortable and safe pedestrian spaces.

Sidewalks

A continuous sidewalk should be provided along both sides of street throughout the district. The sidewalk should be no less than 6 feet in width.

The city should not widen streets at the expense of the sidewalk and other pedestrian areas. Where street widening is necessary, trees, landscaping and sidewalks should be protected wherever possible and the negative impacts of widening should be mitigated.



Cross section view of reverse frontage road sidewalk

The pedestrian walkway adjacent to 135th Street should be separated from the trafficway by a tree lawn which is at least 4 feet in width and/or parallel curb-side parking.

The sidewalk design should encourage the use of street furniture, tree planting, and landscaping in amenity zones, such as at street intersections where pedestrian crossing is to occur.

Mark crosswalks with a change of paving material, color, texture, or pattern which contrasts with the trafficway material to help define crosswalks as areas requiring special attention from both motorists and pedestrians.

Street Furniture

When possible elements of the streetscape such as lighting, traffic controls, signage, benches and other elements should have a consistent visual theme.

Signage

Design signage to be compatible with the surroundings and function.

Traffic control signs should be consolidated and located in an organized way and should be sized to be consistent with traffic speeds and other streetscape elements.

Utilities

When possible utilities should be kept underground. Above ground utility equipment, such as utility boxes within the street right-of-way shall be designed to blend into the street furniture design.

Streetscape Landscaping

Trees and large, densely planted tree lawns should be used in the district to reduce the visual scale of the street, encourage slower traffic, and improve the pedestrian environment.

Use street trees which create an image based on linear forms and become a design element of the streetscape.

Use street trees which mature to form shade canopies. Use trees of a similar species to provide consistency along a block face.



Trees add visual appeal to streetscape

Landscaping on the reverse frontage and north-south access roads which visually narrow the perceived trafficway space, and prompt slower traffic speeds, and enhance pedestrian safety is encouraged.

2. Landscaping Plans

A. Buffers.

Intent. Buffers are typically found at the edge of properties and are used to soften parking lots and other paved services and screen undesirable views. Buffer requirements in the district should be uniformly applied to insure a consistent visual appearance.

Buffer yards and screens should be in conformance with the following objectives and with the plant unit requirements outlined in *Appendix A* of this document.

Landscape buffers should be used to soften parking lots and other paved surfaces. They should be provided where parking lots adjoin the public right-of-way and between residential and commercial properties. The character of the buffer will vary based on the intensity of use.

Provide landscaped screens where the intention is to block undesirable views around uses such as service yards, parking lots (where they are adjacent to residential uses). Screens should be dense enough to prevent views.

Planting area should be designed so they do not obstruct positive views of buildings or prevent surveillance.

B. Interior Treatments

Intent. Landscaping which is internal to a site adds to the visual appeal and character of the use. Landscaping can be effectively used to provide pedestrians with a sense of ownership and comfort in a business district.

Interior landscaping should meet the following objectives and the guidelines outlined in *Appendix A* of this document.

Parking lots shall be provided with landscaped islands which may include shade trees, ornamental trees ground cover and other plant materials.

Design plantings to serve a purpose such as establishing space at sidewalks, buffering between walks and trafficways or parking lots, marking entrances, or screening undesirable views.

Require commercial developers to maintain trees and plants they have installed as landscaping.

3. Property Development

A. Architectural Standards

Intent. Consistency in architecture is important in the Market Square districts. The characteristics of buildings in the district give the area identity and character. If there is no definition or articulation of massing, orientation or style of buildings, a district can seem disjointed or visually confusing. These standards address the following objectives:

- ▶ Create a unified district and a pleasant visual environment
- ▶ Facilitate high quality development
- ▶ Build at a neighborhood scale and create a district which is comfortable to the pedestrian

Detail facades to relate to the architectural characteristics of the surrounding structures such as the proportion, scale, form, mass, setback, material, horizontal dimension, etc.

Where no prevailing architectural context exists, create a compatible theme and character throughout the new development.

Break the mass of large buildings into smaller masses consistent with neighborhood scale development.

Articulate and texture large facades so as to reduce their apparent size and add to the pedestrian scale of the area. At ground level, provide architectural texture and detail on buildings to preserve human scale and the continuity of the streetscape.

Require that each principal building have a clearly defined, highly visible customer entrance with

features such as canopies, porticos, arcades, arches, wing walls and integral planters.

Loading docks, trash collection, outdoor storage and similar facilities and functions shall be incorporated into the overall design of the building and the landscaping so that the visual and acoustic impacts of these functions are fully contained and out of view from adjacent properties and public streets. Use of screening materials that are different from or inferior to the principal materials of the building and landscape is prohibited.

B. Site Planning and Improvement Standards

Intent: Each site in the Mixed-Use Office and Neighborhood Services district should be designed to articulate the pedestrian nature and neighborhood character of the district. The site should function well for both vehicles and pedestrians internally, between adjacent properties and throughout the district.

Building Siting

Create strong continuous corridor edges using either consistent building setbacks or continuous sequences of plant materials, street light standards and compatible signage.

Site new developments to ensure that adjacent properties have visual privacy and sunlight.

2. Parking

Intent. Parking lots are important elements in providing convenience and service to business patrons and employees in the 135th Street corridor district. If parking is poorly conceived it can disrupt the character and visual clarity of the corridor. Parking lots need to be provided in business districts in ways that do not overpower the streetscape, disrupt its character, or interfere with pedestrian activity, yet are still convenient to customers.

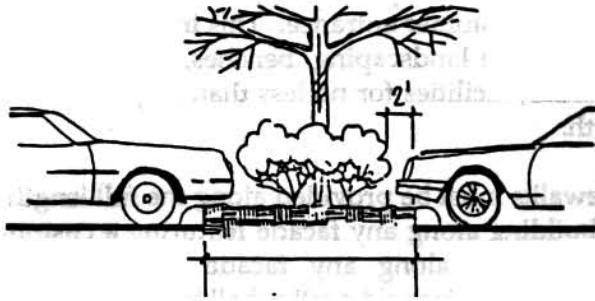
The location of parking lots should be consistent with the general lay-out of a business district. Sidewalks, landscaping and other pedestrian amenities should be provided in parking lots.

Provide on-street parking in the Market Square area. All parking lots shall be paved with a hard surface material. Parking lots and associated landscaped areas shall be surrounded with a 4" curb or other barrier.

Parking lots shall be lighted to provide security, but not produce spill light to residential properties.

Parking lot screening shall, at a minimum follow the standards found in the landscaping and open space section of these standards (*Appendix A*).

Minimize curb cuts and median breaks by requiring adjacent commercial uses to design internal connections between parking lots to minimize street traffic and curb cuts.



Sidewalks:

Intent: All streets in the 135th Street corridor planning area will have sidewalks and appropriately signed pedestrian crosswalks where they are necessary. In order to encourage use of the sidewalk and to provide for pedestrian space, plans and designs shall provide the maximum pedestrian space feasible.

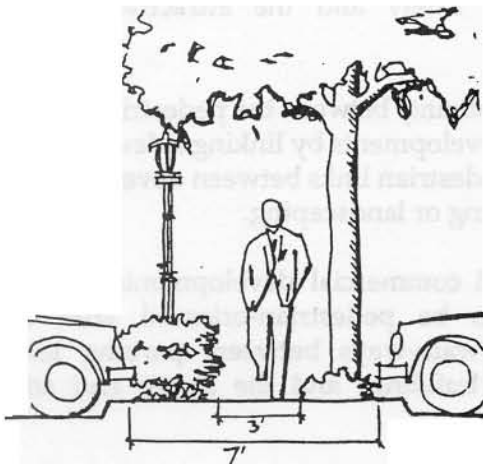
Sidewalks shall be continuous along the entire length of 135th Street, the reverse frontage roads and cross streets.

The sidewalks along 135th Street, the reverse frontage roads and cross streets should connect to the pedestrian circulation system of each development.

Sidewalk design should encourage the use of street furniture, tree planting and landscaping in amenity zones.

Sidewalk design should help identify pedestrian and vehicular separation. Separations can be achieved using trees, landscaping, curb-side parking (on the reverse frontage roads), low walls, hedge rows, setbacks or other devices which provide the pedestrian with a sense of enclosure and containment.

Mark sidewalks with a change of paving material, color, texture or pattern which contrasts with the trafficway material to help define crosswalks as shared territory and as areas requiring special



Separate vehicle/pedestrians

attention from both motorists and pedestrians.

Limit curb cuts to preserve the continuity of the sidewalk space and reduce traffic hazards. Continue sidewalk materials across curb cuts.

A continuous internal pedestrian walkway must be provided from the perimeter public sidewalk to the principal customer entrance. This internal walkway must feature landscaping, benches, and other such materials/facilities for no less than 50 percent of its length.

Sidewalks must be provided along the full length of the building along any facade featuring a customer entrance and along any facade abutting public parking areas. Such sidewalks shall be located at least six feet from the facade of the building to provide planting beds for foundation landscaping.

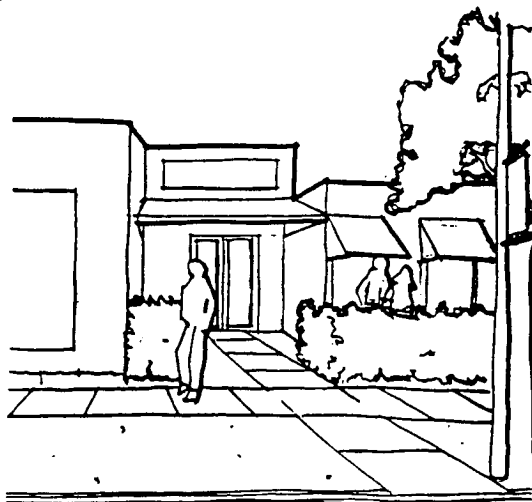
Internal pedestrian walkways must provide a weather protection feature such as an awning within 15 feet of all customer entrances.

The internal pedestrian walkways must be distinguished from driving surfaces through the use of special pavers, bricks, or scored concrete to enhance pedestrian safety and the attractiveness of the walkways.

Provide continuity between the pedestrian systems of adjacent developments by linking sidewalks. Do not obstruct pedestrian links between development with walls, fencing or landscaping.

Require all commercial developments in business districts to be pedestrian-oriented with clearly identified walkways between: parking lots and buildings; buildings and the street; and adjacent properties.

Provide seating at major destination points, points of highest pedestrian volume. Seating should be placed adjacent to, but not in, main traffic flow corridor. Screen seating areas from moving automobile traffic. Emphasize each seating area with appropriate pedestrian scale amenity.



Connect primary building entrance to pedestrian circulation system

Each retail establishment must contribute to the establishment or enhancement of the community and public spaces by providing at least one community amenity such as a patio/seating area, water feature, clock tower, and pedestrian plaza with benches.

Utilities

All new utilities along the 135th Street corridor shall be underground. Utility boxes or any above ground apparatus shall be shielded from view or designed to fit into the landscape or architectural design.

Lighting: Lighting in the 135th Street corridor should provide security and character. It should encourage pedestrian activity over extended hours of the day. The corridor should be lit to increase vehicle and pedestrian safety and to highlight special elements of the streetscape.

Lighting should meet the standards established in *Appendix B* of this document.

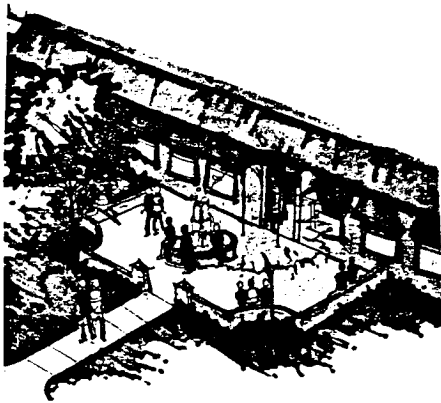
Ornamental and decorative lighting which highlights significant design elements of a business district such as prominent buildings or gateway identification is encouraged.

Parking lots should be lit brightly enough to preserve safety, but without glare, hot spots, or spill light through residential windows. Shielded lights which direct illumination onto the surface to be lit and reduce glare and spill light.

Signs: Signage will be a major component of the visual landscape of the 135th Street corridor. Signs are needed to advertise businesses, direct people, communicate traffic information and set the tone for the Market Square district. Signs can add significantly to the character, interest and visual clarity of a business district. Poorly designed, inappropriate signs, however, can have a negative impact on an area.

Signage should complement buildings and other streetscape elements and be designed of appropriate size, style and materials, and be located to fit properly into the business district. Signage will be review as a primary component of projects.

Sign design should relate to the general theme of the



Create amenity zones

surrounding districts. Design signage to be compatible with its surroundings and function with land use coordinated graphic systems wherever possible.

Roof signs, large overhanging signs, and excessively large signs which interfere with the visual character of a business district shall not be used.

Traffic control signs shall be located in an organized way and shall be sized to be consistent with the traffic speeds which are appropriate to the street.

Consolidate signs wherever possible in a well-organized and clearly understandable manner.

Place and size signs on buildings in keeping with the scale and size of the building facades and general streetscape so as not to obscure or interfere with architectural lines and details.

The development and location of permanent identity signs is encouraged in prominent gateway locations and should include landscaping or other types of additional amenities to highlight these gateways.

Bonuses

- ▶ Buffer yards which exceed the requirements outlined in *Appendix A* by more than 30 plant units.
- ▶ Buffer yards should be integrated into the site design and create a unique design statement significantly beyond the requirements of these standards.
- ▶ Interior landscaping should be encouraged which exceeds the requirements outlined in *Appendix A* by more than 30 plant units.
- ▶ Interior landscaping which is well integrated into the site design, such that: it emphasizes the buildings entry in a positive manner, it significantly adds to pedestrian comfort, and it is used to help create a design theme for the project.
- ▶ Ground level spaces in office buildings should be devoted to retail uses, and shops with display windows, and doors leading directly to the sidewalk.

- ▶ Construct all four sides of buildings to the same standard of design and internalize loading docks, trash collection, outdoor storage and similar facilities and functions within the structure or create a service court which is completely enclosed by the building mass.
- ▶ Provide exterior weather protection for pedestrians outside the main entrance to all structures, such as canopies, porticos, arcades, or free-standing structures. The exterior covering should extend at least six feet from the front edge of the building.
- ▶ Locate a building mass at the front setback line for more than 60 % of the property width. Define the entire length of the front setback line with either building placement or landscaping.
- ▶ Provide a continuous landscaped separation between principal internal walkways and parking areas.
- ▶ Locate more than 50% of the parking behind the development principal structure allowing the building to be sited at the minimum setback line.
- ▶ Provide a parking structure. (Must meet design standards)
- ▶ Locate retail shops at the ground level of parking structures.
- ▶ Contribute one additional community amenity such as a patio seating area, water feature, pedestrian shelter and pedestrian plaza with benches.
- ▶ One continuous pedestrian walkway is provided from the street to the principal building entry with an irrigated planting area or areas landscaped with trees and shrubs provided directly adjacent to the walkway along more than 60% of its length.